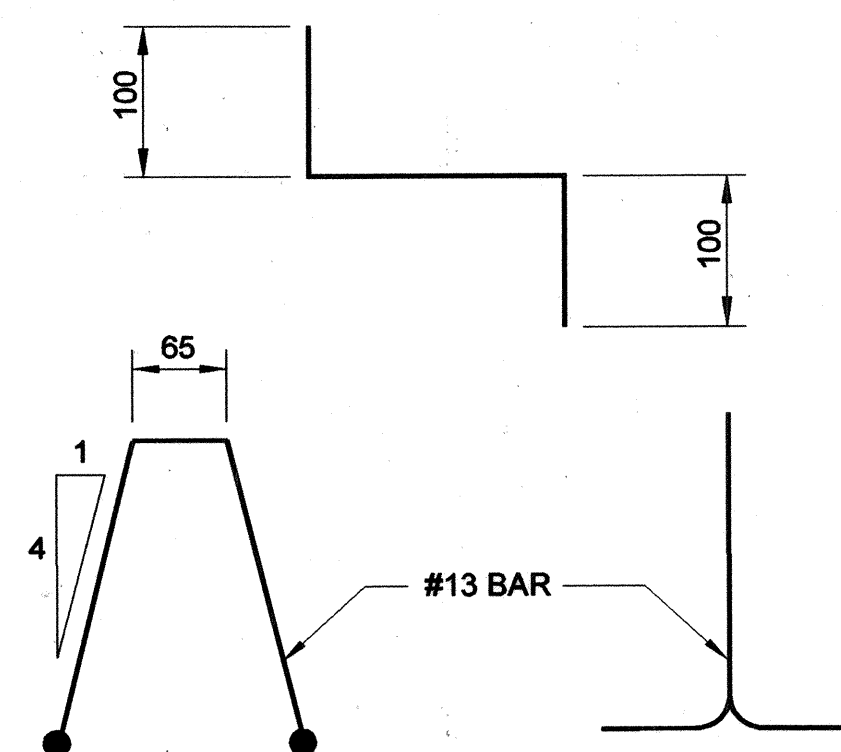
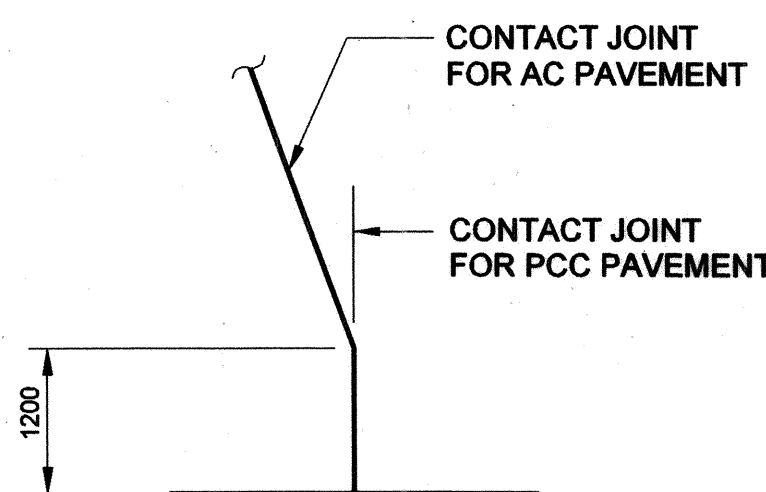


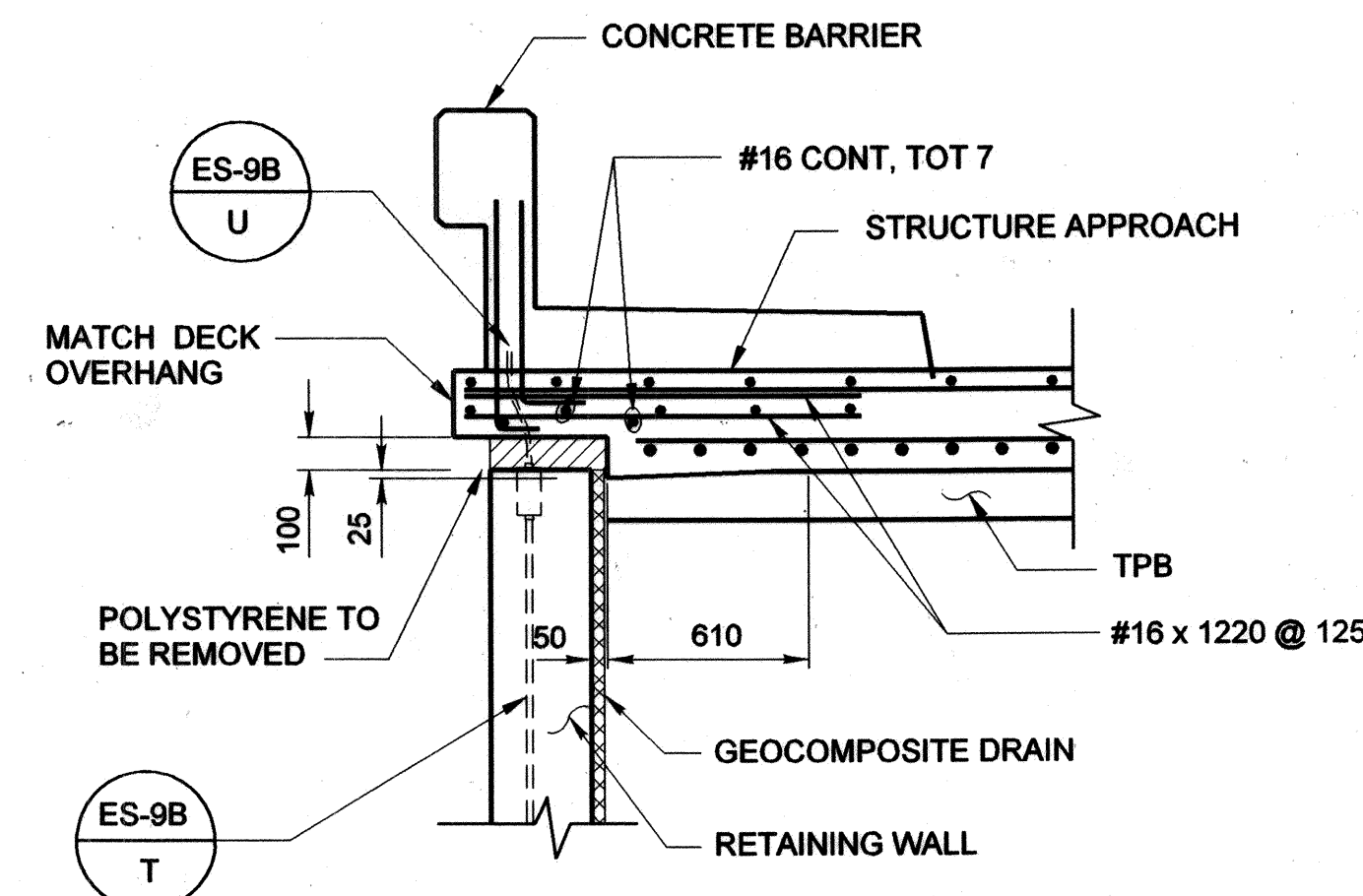
PLAN
Not To Scale



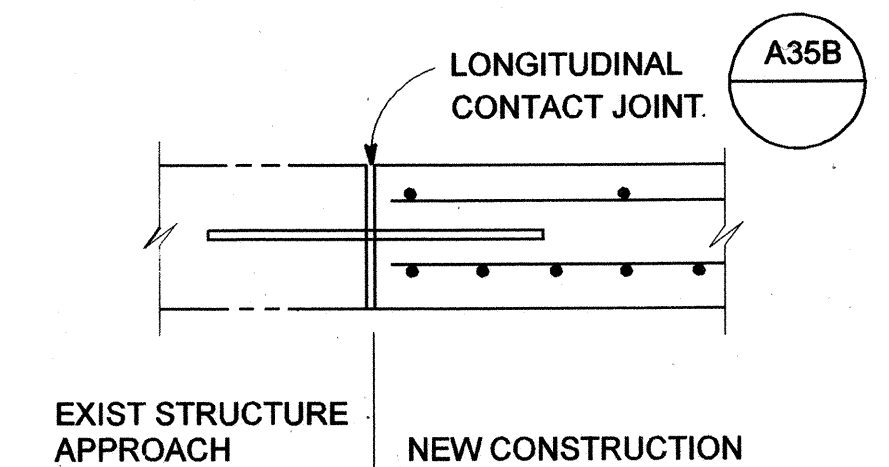
BAR CHAIR DETAIL
1:5



DETAIL A
1:5



SECTION B-B
1:5



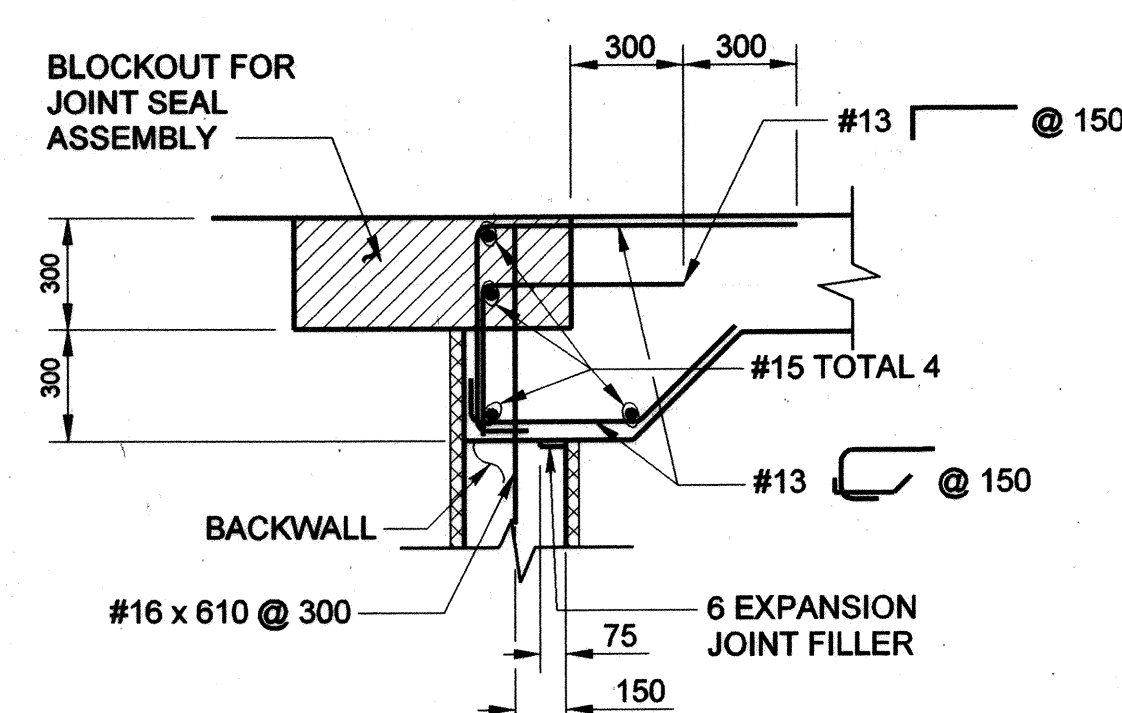
APPROACH SLAB TRANSVERSE CONTACT JOINT		
APPROACH SKEW	WITH AC ROADWAY PAVEMENT	WITH PCC ROADWAY PAVEMENT
< 20°	PARALLEL TO FACE OF PAVING NOTCH	PARALLEL TO FACE OF PAVING NOTCH

LEGEND

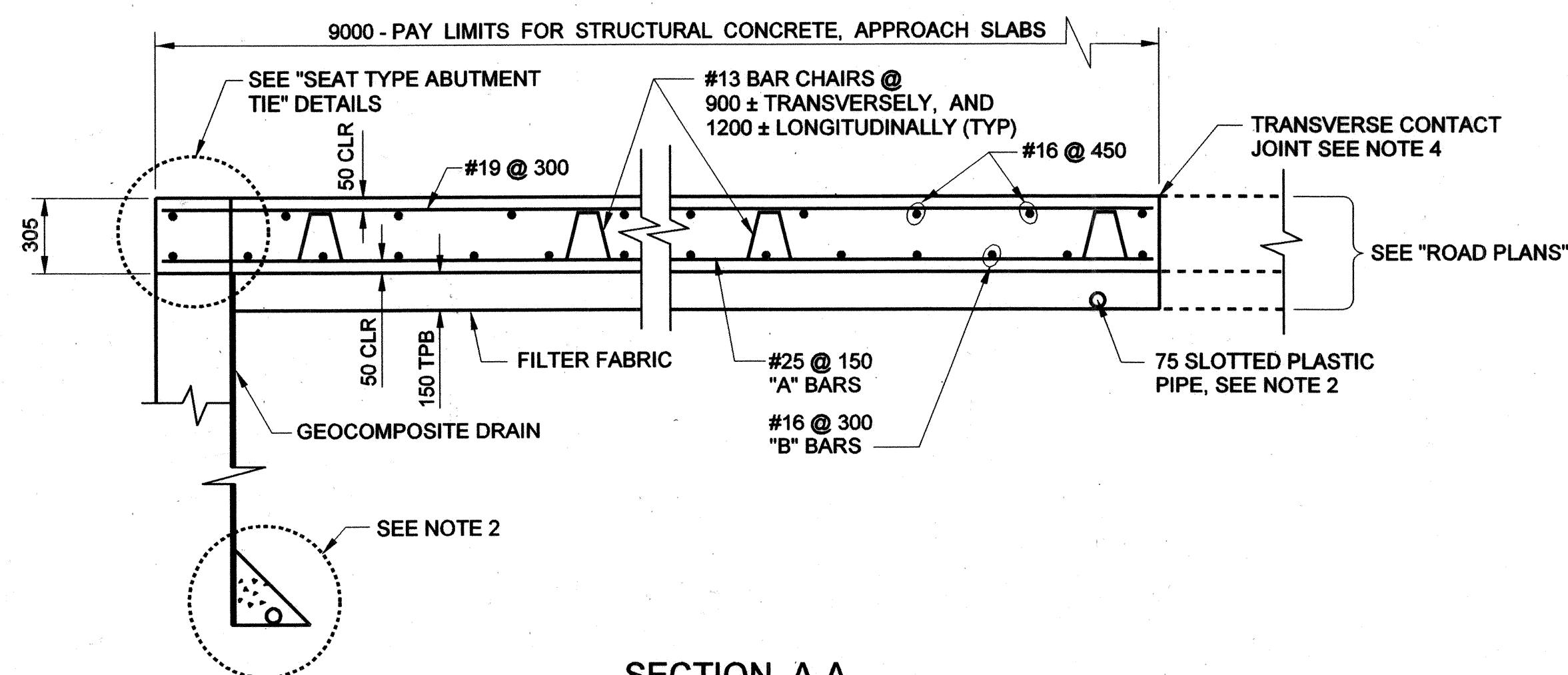
REMOVE ALL POLYSTYRENE.

NOTES:

- FOR DETAILS NOT SHOWN OR NOTED, SEE STRUCTURE PLANS.
- FOR DRAINAGE DETAILS, SEE "STRUCTURE APPROACH DRAINAGE DETAILS" SHEET.
- LONGITUDINAL CONSTRUCTION JOINTS, WHEN PERMITTED BY THE ENGINEER, SHALL BE LOCATED ON LANE LINES AND / OR AT EDGES OR UTILITY VAULTS.
- FOR TRANSVERSE CONTACT JOINT WITH NEW PCC PAVING, REFER TO CALTRANS STANDARD PLAN A35-A.
- END ANGLE AT BEGINNING OF BARRIER TRANSITION, END OF RETAINING WALL OR END OF STRUCTURE APPROACH, AS APPLICABLE.
- AT THE CONTRACTOR'S OPTION, APPROACH SLAB TRANSVERSE REINFORCEMENT MAY BE PLACED PARALLEL TO PAVING NOTCH. SPACING OF TRANSVERSE REINFORCEMENT IS MEASURED ALONG Q ROADWAY.

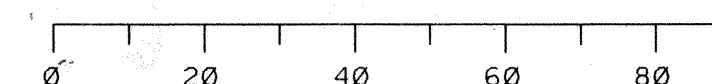


SEAT TYPE ABUTMENT
TIE DETAILS (SEE NOTE 1)
1:5



SECTION A-A
1:5

FOR REDUCED PLANS ORIGINAL
SCALE IS IN MILLIMETERS



Washington
Washington Infrastructure Services, Inc.
17300 Redhill Avenue, Suite 150
Irvine, CA 92614

		ORANGE COUNTY PUBLIC FACILITIES AND RESOURCES DEPARTMENT	
		CROWN VALLEY PARKWAY BRIDGE (WIDENING) OVER ARROYO TRABUCO STRUCTURE APPROACH TYPE N(9S)	
MARK	DATE	DESCRIPTION	
REVISIONS			
PREPARED UNDER RESPONSIBLE CHARGE OF:			
DESIGNED	H. YEE	CHECKED	J. SARAH
DRAWN	R. DANLEY	DATE	MAY 01, 2003
SCALE	AS SHOWN	DRAWING NO.	
SHEET		51	
OF 55			

File Name: M:\26144\Struc\STRAPP1B.DGN
Date Plotted: 7/23/2003 11:22:30

M:\26144\Struc\STRAPP1B.DGN 7/23/2003 11:22:30

Last modified 30-Apr-1998